

2-DIST
04

B.I.N.
7VB

STRUCTURES INSPECTION FIELD REPORT

SPECIAL MEMBER INSPECTION

BR. DEPT. NO.
B-07-015=W-04-039

CITY/TOWN BELMONT=WALTHAM	8.-STRUCTURE NO. B07015-7VB-MUN-CUL	11-Kilo. POINT 000.000	90-ROUTINE INSP. DATE May 2, 2022	93*-SPEC. MEMB. INSP. DATE Nov 16, 2023
07-FACILITY CARRIED ST 60 TRAPELO RD	MEMORIAL NAME/LOCAL NAME		27-YR BUILT 1850	106-YR REBUILT 1900
06-FEATURES INTERSECTED WATER BEAVER BROOK	26-FUNCTIONAL CLASS Urban Arterial	DIST. BRIDGE INSPECTION ENGINEER <i>J. Dideo</i>		*YR REHAB'D (NON 106) 0000
43-STRUCTURE TYPE 801 : Masonry Slab	22-OWNER Town Agency	21-MAINTAINER Town Agency	TEAM LEADER T. Leiper <i>[Signature]</i>	
107-DECK TYPE 1 : Concrete Cast-in-Place	WEATHER CLEAR	TEMP. (air) 14°C	TEAM MEMBERS S. GALLAGHER <i>[Signature]</i>	

WEIGHT POSTING *Not Applicable* X

H	3	3S2	Single
N	N	N	N

Actual Posting
Recommended Posting

Waived Date: 00/00/0000 EJDMT Date: 00/00/0000

Signs In Place (Y=Yes, N=No, NR=Not Required)
Legibility/Visibility

At bridge		Advance	
E	W	E	W

PLANS (Y/N): N
 (V.C.R.) (Y/N): N
 TAPE#: _____

RATING

Rating Report (Y/N): N Date: ---- Recommend for Rating or Rerating (Y/N): N

If YES please give priority:
HIGH () MEDIUM () LOW ()

Inspection data at time of existing rating
I 58: - I 59: - I 60: - I 62: - Date: 00/00/0000

REASON: *[Signature]*

SPECIAL MEMBER(S):

	MEMBER	CRACK (Y/N):	WELD'S CONDITION (0-9)	LOCATION OF CORROSION, SECTION LOSS (%), CRACKS, COLLISION DAMAGE, STRESS CONCENTRATION, ETC.	CONDITION		INV. RATING OF MEMBER FROM RATING ANALYSIS			Deficiencies
					PREVIOUS (0-9)	PRESENT (0-9)	H-20	3	3S2	
A	Item 58.2 - Deck Condition	N	N	See remarks in comments section.	3	3	Not Rated			S-A
B	Item 59.4 - Girders or Beams	N	N	See remarks in comments section.	3	3	Not Rated			S-A
C										
D										
E										

List of field tests performed:
Hands on inspection

	I-58	I-59	I-60	I-62
(Overall Previous Condition)	3	3	4	-
(Overall Current Condition)	3	3	4	-

DEFICIENCY: A defect in a structure that requires corrective action.

CATEGORIES OF DEFICIENCIES:

M= Minor Deficiency Deficiencies which are minor in nature, generally do not impact the structural integrity of the bridge and could easily be repaired. Examples include but are not limited to: Spalled concrete, Minor pot holes, Minor corrosion of steel, Minor scouring, Clogged drainage, etc.

S= Severe/Major Deficiency Deficiencies which are more extensive in nature and need more planning and effort to repair. Examples include but are not limited to: Moderate to major deterioration in concrete, Exposed and corroded rebars, Considerable settlement, Considerable scouring or undermining, Moderate to extensive corrosion to structural steel with measurable loss of section, etc.

C-S= Critical Structural Deficiency A deficiency in a structural element of a bridge that poses an extreme unsafe condition due to the failure or imminent failure of the element which will affect the structural integrity of the bridge.

C-H= Critical Hazard Deficiency A deficiency in a component or element of a bridge that poses an extreme hazard or unsafe condition to the public, but does not impair the structural integrity of the bridge. Examples include but are not limited to: Loose concrete hanging down over traffic or pedestrians, A hole in a sidewalk that may cause injuries to pedestrians, Missing section of bridge railing, etc.

URGENCY OF REPAIR:

I = Immediate- [Inspector(s) Immediately contact District Bridge Inspection Engineer (DBIE) to report the Deficiency and to receive further instruction from him/her].

A = ASAP- [Action/Repair should be initiated by District Maintenance Engineer or the Responsible Party (if not a State owned bridge) upon receipt of the Inspection Report].

P = Prioritize- [Shall be prioritized by District Maintenance Engineer or the Responsible Party (if not a State owned bridge) and repairs made when funds and/or manpower is available].

X=UNKNOWN N=NOT APPLICABLE H=HIDDEN/INACCESSIBLE R=REMOVED

CITY/TOWN BELMONT=WALTHAM	B.I.N. 7VB	BR. DEPT. NO. B-07-015=W-04-039	8.-STRUCTURE NO. B07015-7VB-MUN-CUL	INSPECTION DATE NOV 16, 2023
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REMARKS

BRIDGE ORIENTATION

Structure B-07-015=W-04-039 (7VB) carries ST 60 Trapelo Road over Beaver Brook along the border line between the Town of Belmont and the City of Waltham. The approaches are west and east and the elevations are south and north. Beaver Brook flows from north to south.

GENERAL REMARKS

There are multiple superstructure types for this bridge. There are sixteen (16) granite slabs labeled Slab 1 through 16 from south to north. There are eight (8) steel beams labeled Beam 1 through 8 between Slabs 4 and 5, with a utility main at Beam 5. There is a concrete slab with stay-in-place forms between Slabs 12 and 13. North of Slab 16 there is a concrete extension. The original abutments are comprised of dry laid stone at the south side, and the bridge was widened using cast-in-place concrete abutments on the north side. There is a bituminous wearing surface that has been retrofitted with steel roadway plates that span the length of the superstructure (**see sketches 1 - 3**).

ITEM 58 - DECK

Item 58.2 - Deck Condition

The roadway plates are designated as P1 through P6 from south to north (**see sketch 1**).

There are isolated fully broken tack welds between roadway plates. The plates with broken tack welds typically deflect under live load (**see photo 1**).

There is typically minor scrapes and gouges on the leading edge of the roadway plates.

There are isolated transverse hairline cracks in the approaches adjacent to the plates.

There is a depressed bituminous patch with radial cracks, up to 1/8", at the northeast corner of P5.

ITEM 59 - SUPERSTRUCTURE

Item 59.4 - Girders or Beams

Granite Slabs:

There are isolated longitudinal and diagonal hairline cracks, some with efflorescence near the abutments. There are several up to full width transverse hairline cracks, some with efflorescence near mid-span (**see sketch 2**).

The southwest corner of Slab 1 is unsupported for 6".

Slab 4: Diagonal hairline crack at the east end (**see photo 2**).

Slab 6; Transverse split with previous water leakage noted near midspan (**see photo 3**).

Slab 7; Transverse split near mid-span, up to 2" wide x full depth, with a 20" long x 13" wide x 2" deep spall at the south edge (**see photo 4**).

Slabs 6 & 7 are monitored with crack gauges. The crack gauges indicated little to no movement since installation. The crack gauge at Slab 7 read 0.5 mm from zero (**see photos 5 & 6**).

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REMARKS

Steel I-Beams:

There is timber shielding between the steel I-beams preventing inspection of the web and top flanges. The beams typically exhibit minor rotation about the longitudinal axis, resulting in the bottom flanges no longer being level (**see photo 7**).

There is typically heavy rust and laminated rust throughout the beams with many having up to 1/4" remaining and down to 1/16" remaining. The top and bottom flanges of isolated beams near the abutments and at mid-span have up to 100% section loss x full width (**see sketch 3 and photos 8 & 9**).

Reinforced Concrete Slab:

The reinforced concrete slab which acts as part of the superstructure below the westbound roadway is missing 50% of the stay in place forms on the north side of the slab.

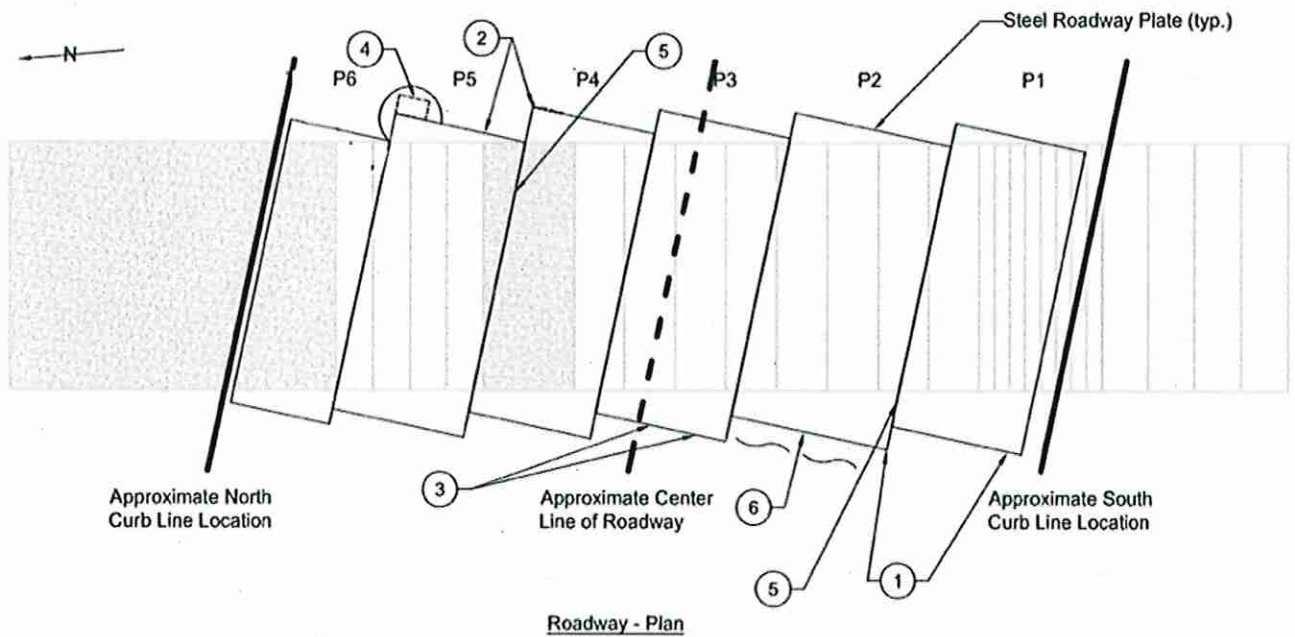
There are two spalls with exposed rebar along the south edge measuring up to 2'-6" long x 9" high x 6" wide. There is a 24" long x 16" wide delamination at the northwest corner.

Sketch / Photo Log

- Sketch 1 : Roadway Plate Conditions
- Sketch 2 : Superstructure Conditions
- Sketch 3 : Steel Beam Conditions
- Photo 1 : Typical condition of the steel plates. Note the height difference between P1 & P2 due to broken welds.
- Photo 2 : Granite Slab 4, there is a diagonal hairline crack at the east end.
- Photo 3 : Granite Slab 6 - Split at mid-span.
- Photo 4 : Granite Slab 7 - Split at mid-span.
- Photo 5 : Granite Slab 6 - Split crack gauge.
- Photo 6 : Granite Slab 7 - Split crack gauge.
- Photo 7 : The beams typically exhibit minor rotation about the longitudinal axis, resulting in the bottom flanges no longer being level. Beams 1 - 3 shown. Note the timber shielding in between beams.
- Photo 8 : Typical heavy section loss and lamintination to the steel at the east end bottom flanges Northern side of Beam 8 shown.
- Photo 9 : Heavy section loss up to 100% at the west end bottom flanges of Beams 7 (left) & Beam 8 (right).

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SKETCHES



Legend:

- Spall (typ.)
- Delamination (typ.)
- Patch
- Hairline Crack (U.O.N)
- Hairline Crack with Efflorescence (U.O.N)

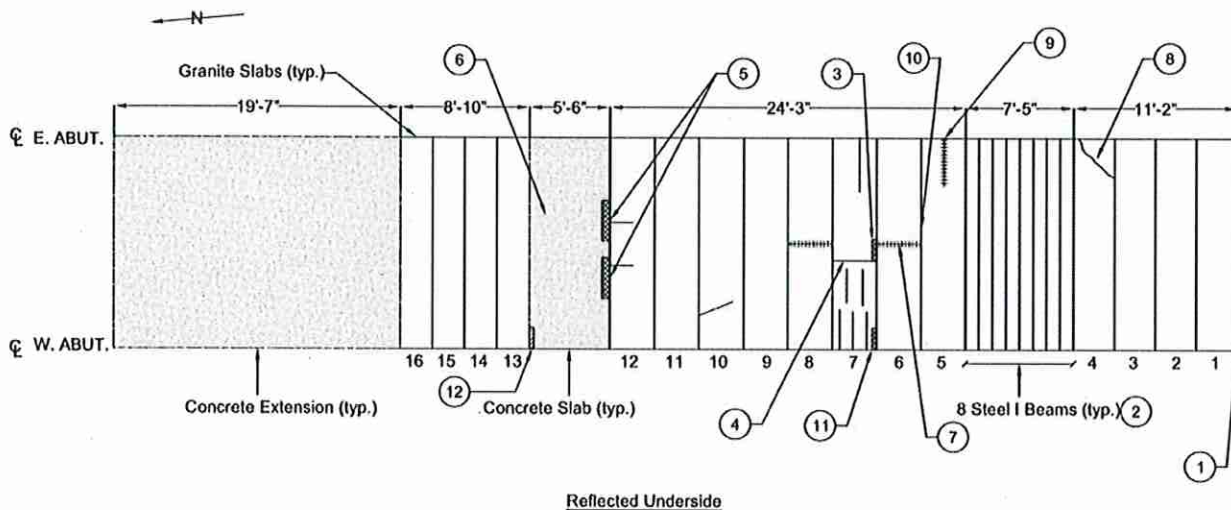
Condition:

1. Southwest corner deflects under live load
2. Northeast corner deflects under live load
3. Minor spalls adjacent to roadway plate
4. 2'-0" W x 7" L depressed bituminous patch with radial cracks $\frac{1}{8}$ " W
5. Full length broken tack weld (typ. 5" L)
6. Minor scrapes and gouges (typ.)

Sketch 1: Roadway Plate Conditions

CITY/TOWN BELMONT=WALTHAM	B.I.N. 7VB	BR. DEPT. NO. B-07-015=W-04-039	8.-STRUCTURE NO. B07015-7VB-MUN-CUL	INSPECTION DATE NOV 16, 2023
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SKETCHES



Legend:

- Spall (typ.)
- Delamination (typ.)
- Hairline Crack (U.O.N)
- Hairline Crack with Efflorescence (U.O.N)

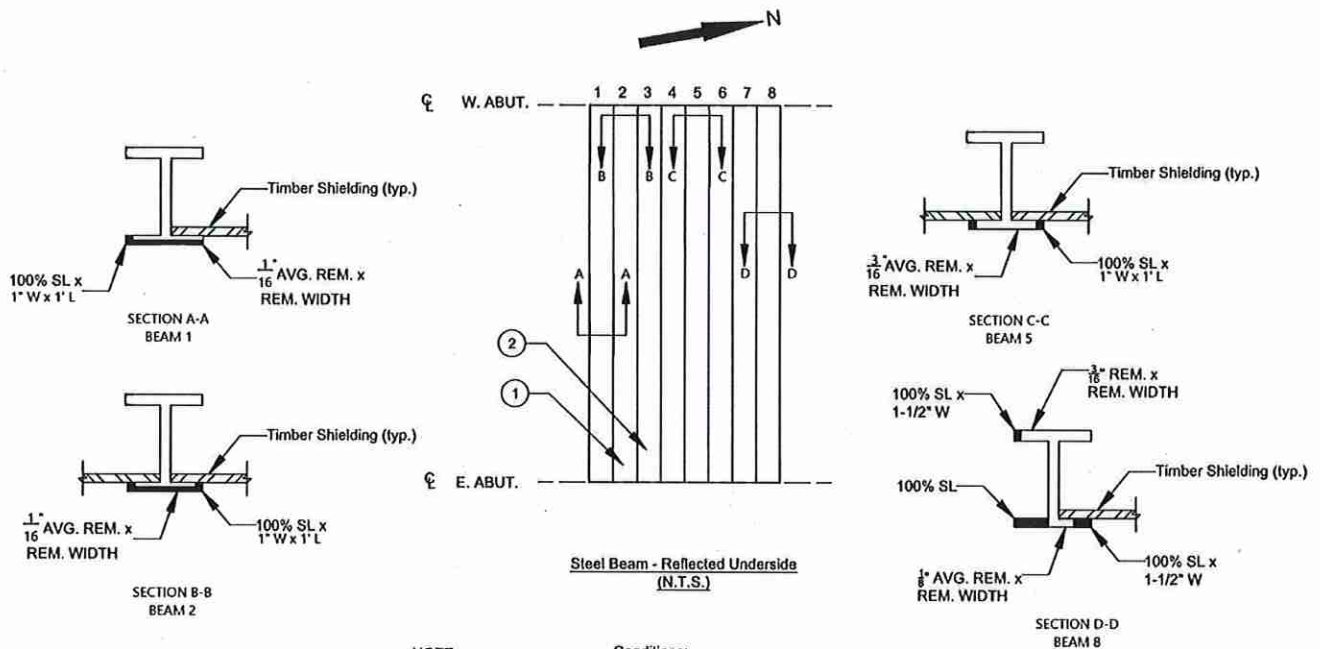
Condition:

1. Granite Slab end unsupported at the SW corner for 6"
2. See Sketch 3: Steel Beam Conditions
3. Granite Slab with a 20" L x 13" W x 2" D spall
4. Granite slab with up to 2" W x full depth crack. Crack gauge reads 0.05mm
5. Two (2) up to 30" L x 6" W x up to 9" D edge spall with exposed rebar
6. 50% of stay in place forms are missing
7. Full width hairline crack with efflorescence. Note: Crack gauge reads 0.00mm
8. 6'-0" L hairline crack
9. 2'-6" L hairline crack with minor efflorescence
10. Loose pointing between the beams
11. 2'-0" L x 5" W x 2" D spall
12. 2'-0" L x 16" W delamination

Sketch 2: Superstructure Conditions

CITY/TOWN BELMONT=WALTHAM	B.I.N. 7VB	BR. DEPT. NO. B-07-015=W-04-039	8.-STRUCTURE NO. B07015-7VB-MUN-CUL	INSPECTION DATE NOV 16, 2023
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SKETCHES



NOTE:
Interior web and top flange inaccessible due to timber shielding. Beams bottom flanges are typically rotated.

Conditions:

1. Bottom Flange: 100% section loss x 1" W at the south toes, 3/16" average remaining x remaining width
2. Bottom Flange: 100% section loss x 1/2" W at both toes, 1/8" average remaining x remaining width

Sketch 3: Steel Beam Conditions

CITY/TOWN BELMONT=WALTHAM	B.I.N. 7VB	BR. DEPT. NO. B-07-015=W-04-039	8.-STRUCTURE NO. B07015-7VB-MUN-CUL	INSPECTION DATE NOV 16, 2023
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PHOTOS

Photo 1: Typical condition of the steel plates. Note the height difference between P1 & P2 due to broken welds.

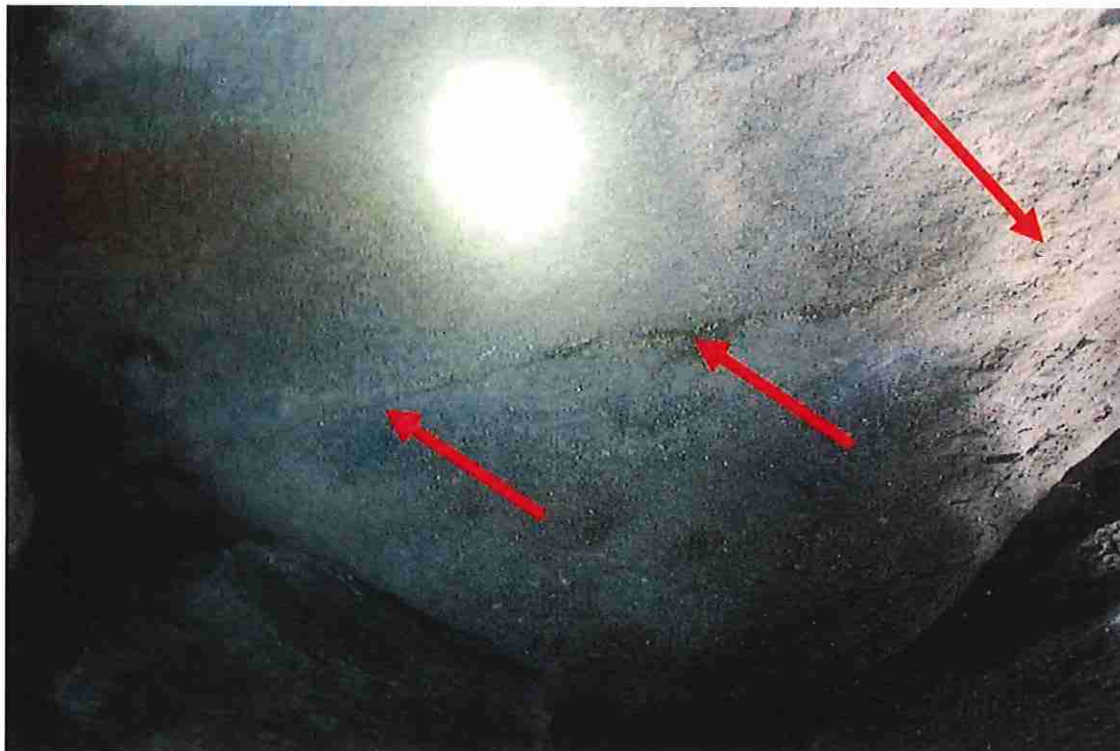


Photo 2: Granite Slab 4, there is a diagonal hairline crack at the east end.

CITY/TOWN BELMONT=WALTHAM	B.I.N. 7VB	BR. DEPT. NO. B-07-015=W-04-039	8.-STRUCTURE NO. B07015-7VB-MUN-CUL	INSPECTION DATE NOV 16, 2023
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PHOTOS



Photo 3: Granite Slab 6 - Split at mid-span.



Photo 4: Granite Slab 7 - Split at mid-span.

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PHOTOS

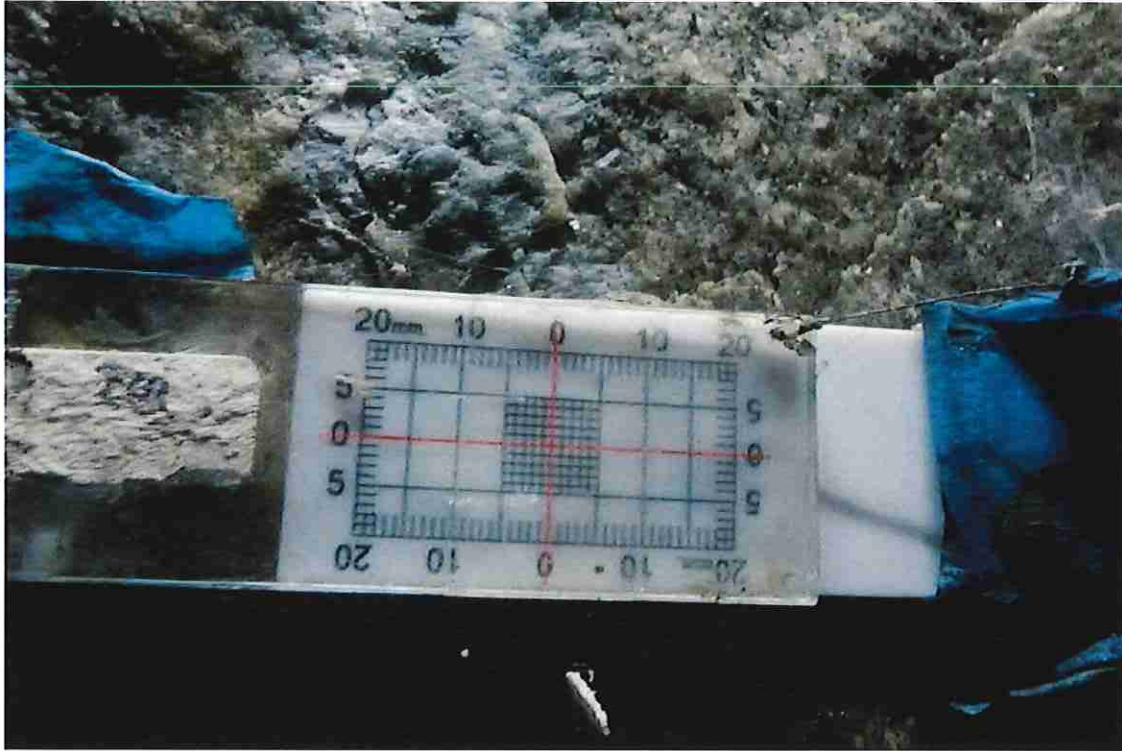


Photo 5: Granite Slab 6 - Split crack gauge.

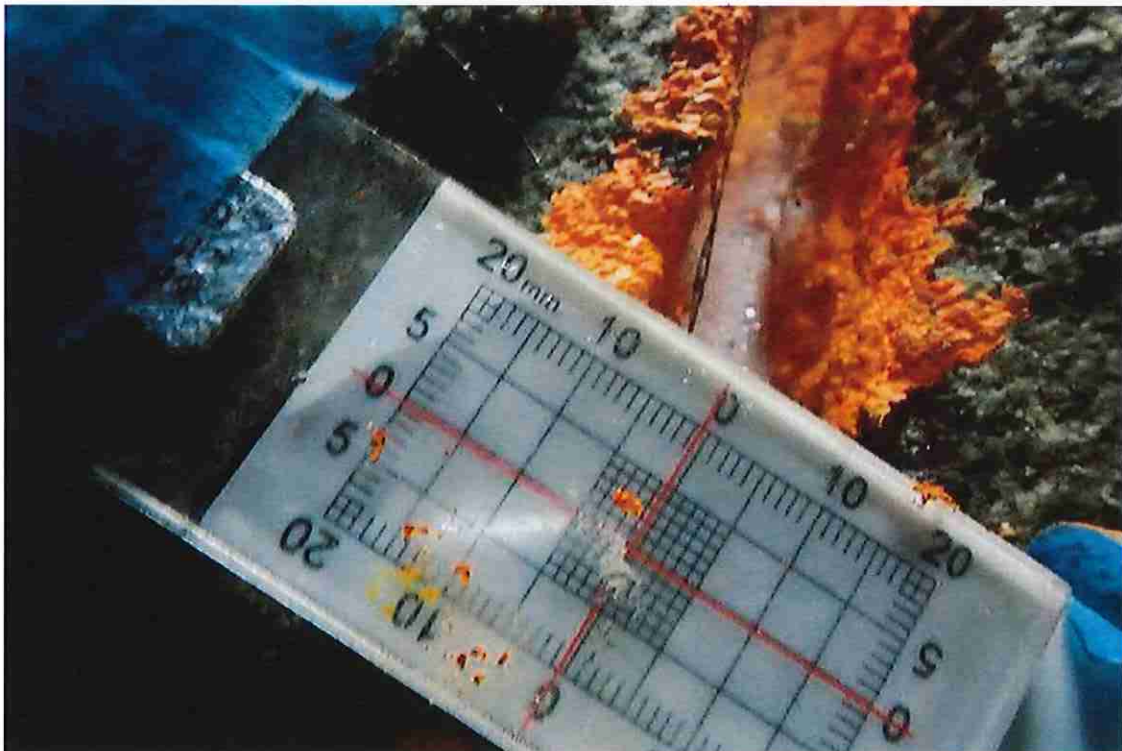


Photo 6: Granite Slab 7 - Split crack gauge.

CITY/TOWN BELMONT=WALTHAM	B.I.N. 7VB	BR. DEPT. NO. B-07-015=W-04-039	8.-STRUCTURE NO. B07015-7VB-MUN-CUL	INSPECTION DATE NOV 16, 2023
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PHOTOS



Photo 7: The beams typically exhibit minor rotation about the longitudinal axis, resulting in the bottom flanges no longer being level. Beams 1 - 3 shown. Note the timber shielding in between beams.



Photo 8: Typical heavy section loss and lamintination to the steel at the east end bottom flanges Northern side of Beam 8 shown.

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PHOTOS

Photo 9: Heavy section loss up to 100% at the west end bottom flanges of Beams 7 (left) & Beam 8 (right).

State Information				Classification	Code
BDEPT#= B07015=W04039	Agency Br.No.	(112) NBIS Bridge Length			N
Town= Belmont=Waltham	L.O.	(104) Highway System			N
B.I.N= 7VB	AASHTO= 002.0	(26) Functional Class -	Urban Arterial		14
RANK= 0 H.I.= 90.6 %	FHWA Select List= N (6/21/2017)	(100) Defense Highway			1
(8) Structure Number	B070157VBMUNCUL	(101) Parallel Structure			N
(5) Inventory Route	200000000	(102) Direction of Traffic -	2-way traffic		2
(2) State Highway Department District	04	(103) Temporary Structure			Y
(3) County Code 017 (4) Place code	05070	(105) Federal Lands Highways			0
(6) Features Intersected	WATER BEAVER BROOK	(110) Designated National Network			N
(7) Facility Carried	ST 60 TRAPELO RD	(20) Toll -	On free road		3
(9) Location		(21) Maintain -	Town Agency		03
(11) Kilometerpoint	0000.000	(22) Owner -	Town Agency		03
(12) Base Highway Network	N	(37) Historical Significance	undetermined		
(13) LRS Inventory Route & Subroute	.000000000000	Condition			Code
(16) Latitude	42 DEG 23 MIN 25.94 SEC	(58) Deck			3
(17) Longitude	71 DEG 11 MIN 48.22 SEC	(59) Superstructure			3
(98) Border Bridge State Code	Share %	(60) Substructure			4
(99) Border Bridge Structure No. #		(61) Channel & Channel Protection			5
(43) Structure Type Main: Masonry	Code 801	(62) Culverts			N
Slab	Jointless bridge type: Not applicable	Load Rating and Posting			Code
(44) Structure Type Appr: Other	Code 000	(31) Design Load -	Unknown		0
(45) Number of spans in main unit	001	(63) Operating Rating Method -	Allowable Stress (AS)		2
(46) Number of approach spans	0000	(64) Operating Rating			00.0
(107) Deck Structure Type - Concrete Cast-in-Place	Code 1	(65) Inventory Rating Method -	Allowable Stress (AS)		2
(108) Wearing Surface / Protective System:		(66) Inventory Rating			00.0
A) Type of wearing surface - Bituminous	Code 6	(70) Bridge Posting			0
B) Type of membrane - Unknown	Code 8	(41) Structure -	Open with shoring		D
C) Type of deck protection - None	Code 0	Appraisal			Code
(27) Year Built	1850	(67) Structural Evaluation			2
(106) Year Reconstructed	1900	(68) Deck Geometry			2
(42) Type of Service: On - Highway-Ped		(69) Underclearances, vert. and horiz.			N
Under - Waterway	Code 55	(71) Waterway adequacy			7
(28) Lanes: On Structure 04 Under structure	Code 00	(72) Approach Roadway Alignment			7
(29) Average Daily Traffic	033000	(36) Traffic Safety Features			0 0 0 0
(30) Year of ADT 2018 (109) Truck ADT	08 %	(113) Scour Critical Bridges			6
(19) Bypass, detour length	005 KM	Inspections			
(48) Length of maximum span	0002.0 M	(90) Inspection Date 05/02/22	(91) Frequency 24 MO		
(49) Structure Length	00002.9 M	(92) Critical Feature Inspection:	(93) CFI DATE		
(50) Curb or sidewalk: Left 02.7 M Right 00.0 M		(A) Fracture Critical Detail	N 00 MO A) 00/00/00		
(51) Bridge Roadway Width Curb to Curb	013.3 M	(B) Underwater Inspection	Y 12 MO B) 11/29/21		
(52) Deck Width Out to Out	023.7 M	(C) Other Special Inspection	Y 06 MO C) 11/14/22		
(32) Approach Roadway Width (w/shoulders)	013.3 M	(* Other Inspection ()	N 00 MO *) 00/00/00		
(33) Bridge Median - No median	Code 0	(* Closed Bridge	N 00 MO *) 00/00/00		
(34) Skew 00 DEG (35) Structure Flared	N	(* UW Special Inspection	N 00 MO *) 01/13/17		
(10) Inventory Route MIN Vert Clear	99.99 M	(* Damage Inspection	MO *) 00/00/00		
(47) Inventory Route Total Horiz Clear	13.3 M	Rating Loads			
(53) Min Vert Clear Over Bridge Rdwy	99.99 M	Report Date 00/00/00	H20 Type 3 Type 3S2 Type HS		
(54) Min Vert Underclear ref N	00.00 M	Operating	0.0 0.0 0.0 0.0		
(55) Min Lat Underclear RT ref N	00.0 M	Inventory	0.0 0.0 0.0 0.0		
(56) Min Lat Underclear LT	00.0 M	Field Posting			
Navigation Data		Status	Posting Date 00/00/00		
(38) Navigation Control - No navigation control on waterway	Code 0	Actual	2 Axle 3 Axle 5 Axle Single		
(111) Pier Protection	Code	Recommended			
(39) Navigation Vertical Clearance	000.0 M	Missing Signs N			
(116) Vert-lift Bridge Nav Min Vert Clear	M	Misc.			
(40) Navigation Horizontal Clearance	0000.0 M	Bridge Name	N Anti-missile fence N Acrow Panel N Jointless Bridge		
		Freeze/Thaw N: Not Applicable			
		# Stairs On/Adjacent 0 Stair Owner(s)			
		Accessibility (Needed/Used)			
		N / N Liftbucket	N / N Rigging	N / N Other	
		N / N Ladder	N / N Staging		
		N / N Boat	N / N Traffic Control		
		Y / Y Wader	N / N RR Flagperson	Inspection	
		N / N Inspector 50	N / N Police	Hours: 012	

11/14/22 11/16/23

State Information				Classification	Code
BDEPT#=	B07015=W04039	Agency Br.No.		(112) NBIS Bridge Length	N
Town=	Belmont=Waltham	L.O.		(104) Highway System	N
B.I.N=	7VB	AASHTO=	002.0	(26) Functional Class -	Urban Arterial
RANK=	0	H.I.=	90.6 %	(100) Defense Highway	1
FHWA Select List= N (6/21/2017)				(101) Parallel Structure	N
(8) Structure Number	B070157VBMUNCUL	(102) Direction of Traffic -	2-way traffic	(103) Temporary Structure	Y
(5) Inventory Route	200000000	(105) Federal Lands Highways		(110) Designated National Network	N
(2) State Highway Department District	04	(20) Toll -	On free road	(21) Maintain -	Town Agency
(3) County Code	017	(4) Place code	05070	(22) Owner -	Town Agency
(6) Features Intersected	WATER BEAVER BROOK	(37) Historical Significance	undetermined		
(7) Facility Carried	ST 60 TRAPELO RD	(58) Deck			3
(9) Location		(59) Superstructure			3
(11) Kilometerpoint	0000.000	(60) Substructure			4
(12) Base Highway Network	N	(61) Channel & Channel Protection			5
(13) LRS Inventory Route & Subroute	000000000000	(62) Culverts			N
(16) Latitude	42 DEG 23 MIN 25.94 SEC	(31) Design Load -	Unknown		0
(17) Longitude	71 DEG 11 MIN 48.22 SEC	(63) Operating Rating Method -	Allowable Stress (AS)		2
(98) Border Bridge State Code	Share %	(64) Operating Rating			00.0
(99) Border Bridge Structure No. #		(65) Inventory Rating Method -	Allowable Stress (AS)		2
Structure Type and Material				(66) Inventory Rating	00.0
(43) Structure Type Main:	Masonry	Code	801	(70) Bridge Posting	0
Slab	Jointless bridge type:	Code	Not applicable	(41) Structure -	Open with shoring
(44) Structure Type Appr:	Other	Code	000		D
(45) Number of spans in main unit		Code	001		
(46) Number of approach spans		Code	0000		
(107) Deck Structure Type -	Concrete Cast-in-Place	Code	1		
(108) Wearing Surface / Protective System:					
A) Type of wearing surface -	Bituminous	Code	6		
B) Type of membrane -	Unknown	Code	8		
C) Type of deck protection -	None	Code	0		
Age and Service					
(27) Year Built			1850		
(106) Year Reconstructed			1900		
(42) Type of Service: On -	Highway-Ped				
Under -	Waterway	Code	55		
(28) Lanes: On Structure	04	Under structure	00		
(29) Average Daily Traffic			033000		
(30) Year of ADT	2018	(109) Truck ADT	08 %		
(19) Bypass, detour length			005 KM		
Geometric Data					
(48) Length of maximum span			0002.0 M		
(49) Structure Length			00002.9 M		
(50) Curb or sidewalk:	Left	02.7 M	Right	00.0 M	
(51) Bridge Roadway Width Curb to Curb			013.3 M		
(52) Deck Width Out to Out			023.7 M		
(32) Approach Roadway Width (w/shoulders)			013.3 M		
(33) Bridge Median -	No median	Code	0		
(34) Skew	00 DEG	(35) Structure Flared	N		
(10) Inventory Route MIN Vert Clear			99.99 M		
(47) Inventory Route Total Horiz Clear			13.3 M		
(53) Min Vert Clear Over Bridge Rdwy			99.99 M		
(54) Min Vert Underclear ref	N		00.00 M		
(55) Min Lat Underclear RT ref	N		00.00 M		
(56) Min Lat Underclear LT			00.00 M		
Navigation Data					
(38) Navigation Control -	No navigation control on waterway	Code	0		
(111) Pier Protection		Code			
(39) Navigation Vertical Clearance			000.0 M		
(116) Vert-lift Bridge Nav Min Vert Clear			M		
(40) Navigation Horizontal Clearance			0000.0 M		
Load Rating and Posting					
(31) Design Load -	Unknown				
(63) Operating Rating Method -	Allowable Stress (AS)				
(64) Operating Rating					
(65) Inventory Rating Method -	Allowable Stress (AS)				
(66) Inventory Rating					
(70) Bridge Posting					
(41) Structure -	Open with shoring				
Appraisal					
(67) Structural Evaluation					
(68) Deck Geometry					
(69) Underclearances, vert. and horiz.					
(71) Waterway adequacy					
(72) Approach Roadway Alignment					
(36) Traffic Safety Features			0 0 0 0		
(113) Scour Critical Bridges					6
Inspections					
(90) Inspection Date	05/02/22	(91) Frequency	24 MO		
(92) Critical Feature Inspection:		(93) CFI DATE			
(A) Fracture Critical Detail	N	00 MO A)	00/00/00		
(B) Underwater Inspection	Y	12 MO B)	11/29/21		
(C) Other Special Inspection	Y	06 MO C)	11/16/23		
(*) Other Inspection ()	N	00 MO *)	00/00/00		
(*) Closed Bridge	N	00 MO *)	00/00/00		
(*) UW Special Inspection	N	00 MO *)	01/13/17		
(*) Damage Inspection		MO *)	00/00/00		
Rating Loads					
Report Date	00/00/00	H20	Type 3	Type 3S2	Type HS
Operating	0.0	0.0	0.0	0.0	0.0
Inventory	0.0	0.0	0.0	0.0	0.0
Field Posting					
Status		Posting Date	00/00/00		
Actual	2 Axle	3 Axle	5 Axle	Single	
Recommended					
Missing Signs	N				
Misc.					
Bridge Name	N Anti-missile fence	N Acrow Panel	N Jointless Bridge		
Freeze/Thaw	N : Not Applicable				
# Stairs On/Adjacent	0	Stair Owner(s)			
Accessibility (Needed/Used)					
N / N	Liftbucket	N / N	Rigging	N / N	Other
N / N	Ladder	N / N	Staging		
N / N	Boat	N / N	Traffic Control		
Y / Y	Wader	N / N	RR Flagperson		Inspection
N / N	Inspector 50	N / N	Police		Hours: 012